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**Garage**  
**PRK 2561 BLDG 002**  
Detailed Engineering Evaluation  
Qualitative Report  
Version Final

75 Lower Styx Road, Bottle Lake



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Qualitative Report  
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75 Lower Styx Road, Bottle Lake

Christchurch City Council

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**Date**  
14 May 2013



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# Qualitative Report Summary

## **Garage**

**PRK 2561 BLDG 002**

## **Detailed Engineering Evaluation**

### **Qualitative Report - SUMMARY**

#### **Version Final**

**75 Lower Styx Road, Bottle Lake, Christchurch**

## **Background**

This is a summary of the Qualitative report for the building structure, and is based in part on the Detailed Engineering Evaluation Procedure document (draft) issued by the Structural Advisory Group on 19 July 2011 and visual inspections on 20<sup>th</sup> April 2012.

## **Building Description**

The garage is located at 75 Lower Styx Road, Bottle Lake, Christchurch. It was constructed in 1960. The building is used for storage purposes.

The building is of lightweight timber frame construction with no internal wall linings. Exterior cladding is red brick. The roof structure consists of lightweight metal roofing fixed to timber sarking on timber roof trusses. Foundations are concrete slab on grade.

## **Key Damage Observed**

- No damage was noted to the building

## **Critical Structural Weaknesses**

No potential critical structural weaknesses have been identified for this building.

## **Indicative Building Strength (from IEP and CSW assessment)**

Based on the information available, and using the NZSEE Initial Evaluation Procedure, the original capacity of the building has been assessed to be in the order of 43% NBS and post-earthquake capacity also in the order of 43% NBS.

The building has been assessed to have a seismic capacity in the order of 43% NBS and is therefore considered to be potentially Earthquake Risk.



## **Recommendations**

The recent seismic activity in Christchurch has caused no visible damage to the building. But as the building has achieved less than 67% NBS following a qualitative Detailed Engineering Evaluation of the building, GHD recommend that a more detailed quantitative assessment and geotechnical investigation is carried out and if necessary strengthening options explored.

As the building has achieved greater than 34% NBS, occupation of the building is permitted.



## 1. Background

GHD has been engaged by the Christchurch City Council (CCC) to undertake a detailed engineering evaluation of the garage at 75 Lower Styx Road.

This report is a Qualitative Assessment of the building structure, and is based in part on the Detailed Engineering Evaluation Procedure document (draft) issued by the Structural Advisory Group on 19 July 2011.

A qualitative assessment involves inspections of the building and a desktop review of existing structural and geotechnical information, including existing drawings and calculations, if available.

The purpose of the assessment is to determine the likely building performance and damage patterns, to identify any potential critical structural weaknesses or collapse hazards, and to make an initial assessment of the likely building strength in terms of percentage of new building standard (%NBS).

At the time of this report, no intrusive site investigation, detailed analysis, or modelling of the building structure had been carried out. No drawings were made available and as such, all structural details have been inferred from visual inspections.



## 2. Compliance

This section contains a brief summary of the requirements of the various statutes and authorities that control activities in relation to buildings in Christchurch at present.

### 2.1 Canterbury Earthquake Recovery Authority (CERA)

CERA was established on 28 March 2011 to take control of the recovery of Christchurch using powers established by the Canterbury Earthquake Recovery Act enacted on 18 April 2011. This act gives the Chief Executive Officer of CERA wide powers in relation to building safety, demolition and repair. Two relevant sections are:

#### **Section 38 – Works**

This section outlines a process in which the chief executive can give notice that a building is to be demolished and if the owner does not carry out the demolition, the chief executive can commission the demolition and recover the costs from the owner or by placing a charge on the owners' land.

#### **Section 51 – Requiring Structural Survey**

This section enables the chief executive to require a building owner, insurer or mortgagee carry out a full structural survey before the building is re-occupied.

We understand that CERA will require a detailed engineering evaluation to be carried out for all buildings (other than those exempt from the Earthquake Prone Building definition in the Building Act). It is anticipated that CERA will adopt the Detailed Engineering Evaluation Procedure document (draft) issued by the Structural Advisory Group on 19 July 2011. This document sets out a methodology for both qualitative and quantitative assessments.

The qualitative assessment is a desk-top and site inspection assessment. It is based on a thorough visual inspection of the building coupled with a review of available documentation such as drawings and specifications. The quantitative assessment involves analytical calculation of the buildings strength and may require non-destructive or destructive material testing, geotechnical testing and intrusive investigation.

It is anticipated that factors determining the extent of evaluation and strengthening level required will include:

- ▶ The importance level and occupancy of the building
- ▶ The placard status and amount of damage
- ▶ The age and structural type of the building
- ▶ Consideration of any critical structural weaknesses
- ▶ The extent of any earthquake damage





## **2.2 Building Act**

Several sections of the Building Act are relevant when considering structural requirements:

### **Section 112 – Alterations**

This section requires that an existing building complies with the relevant sections of the Building Code to at least the extent that it did prior to any alteration. This effectively means that a building cannot be weakened as a result of an alteration (including partial demolition).

### **Section 115 – Change of Use**

This section requires that the territorial authority (in this case Christchurch City Council (CCC)) be satisfied that the building with a new use complies with the relevant sections of the Building Code 'as near as is reasonably practicable'. Regarding seismic capacity 'as near as reasonably practicable' has previously been interpreted by CCC as achieving a minimum of 67% NBS however where practical achieving 100% NBS is desirable. The New Zealand Society for Earthquake Engineering (NZSEE) recommend a minimum of 67% NBS.

#### **2.2.1 Section 121 – Dangerous Buildings**

The definition of dangerous building in the Act was extended by the Canterbury Earthquake (Building Act) Order 2010, and it now defines a building as dangerous if:

- ▶ In the ordinary course of events (excluding the occurrence of an earthquake), the building is likely to cause injury or death or damage to other property; or
- ▶ In the event of fire, injury or death to any persons in the building or on other property is likely because of fire hazard or the occupancy of the building; or
- ▶ There is a risk that the building could collapse or otherwise cause injury or death as a result of earthquake shaking that is less than a 'moderate earthquake' (refer to Section 122 below); or
- ▶ There is a risk that that other property could collapse or otherwise cause injury or death; or
- ▶ A territorial authority has not been able to undertake an inspection to determine whether the building is dangerous.

### **Section 122 – Earthquake Prone Buildings**

This section defines a building as earthquake prone if its ultimate capacity would be exceeded in a 'moderate earthquake' and it would be likely to collapse causing injury or death, or damage to other property. A moderate earthquake is defined by the building regulations as one that would generate ground shaking 33% of the shaking used to design an equivalent new building.

### **Section 124 – Powers of Territorial Authorities**

This section gives the territorial authority the power to require strengthening work within specified timeframes or to close and prevent occupancy to any building defined as dangerous or earthquake prone.

### **Section 131 – Earthquake Prone Building Policy**

This section requires the territorial authority to adopt a specific policy for earthquake prone, dangerous and insanitary buildings.



## 2.3 Christchurch City Council Policy

Christchurch City Council adopted their Earthquake Prone, Dangerous and Insanitary Building Policy in 2006. This policy was amended immediately following the Darfield Earthquake of the 4th September 2010.

The 2010 amendment includes the following:

- ▶ A process for identifying, categorising and prioritising Earthquake Prone Buildings, commencing on 1 July 2012;
- ▶ A strengthening target level of 67% of a new building for buildings that are Earthquake Prone;
- ▶ A timeframe of 15-30 years for Earthquake Prone Buildings to be strengthened; and,
- ▶ Repair works for buildings damaged by earthquakes will be required to comply with the above.

The council has stated their willingness to consider retrofit proposals on a case by case basis, considering the economic impact of such a retrofit.

We anticipate that any building with a capacity of less than 33% NBS (including consideration of critical structural weaknesses) will need to be strengthened to a target of 67% NBS of new building standard as recommended by the Policy.

If strengthening works are undertaken, a building consent will be required. A requirement of the consent will require upgrade of the building to comply 'as near as is reasonably practicable' with:

- ▶ The accessibility requirements of the Building Code.
- ▶ The fire requirements of the Building Code. This is likely to require a fire report to be submitted with the building consent application.

## 2.4 Building Code

The building code outlines performance standards for buildings and the Building Act requires that all new buildings comply with this code. Compliance Documents published by The Department of Building and Housing can be used to demonstrate compliance with the Building Code.

After the February Earthquake, on 19 May 2011, Compliance Document B1: Structure was amended to include increased seismic design requirements for Canterbury as follows:

- ▶ Hazard Factor increased from 0.22 to 0.3 (36% increase in the basic seismic design load)
- ▶ Serviceability Return Period Factor increased from 0.25 to 0.33 (80% increase in the serviceability design loads when combined with the Hazard Factor increase)

The increase in the above factors has resulted in a reduction in the level of compliance of an existing building relative to a new building despite the capacity of the existing building not changing.



### 3. Earthquake Resistance Standards

For this assessment, the building's earthquake resistance is compared with the current New Zealand Building Code requirements for a new building constructed on the site. This is expressed as a percentage of new building standard (%NBS). The new building standard load requirements have been determined in accordance with the current earthquake loading standard (NZS 1170.5:2004 Structural design actions - Earthquake actions - New Zealand).

The likely capacity of this building has been derived in accordance with the New Zealand Society for Earthquake Engineering (NZSEE) guidelines 'Assessment and Improvement of the Structural Performance of Buildings in Earthquakes' (AISPBE), 2006. These guidelines provide an Initial Evaluation Procedure that assesses a buildings capacity based on a comparison of loading codes from when the building was designed and currently. It is a quick high-level procedure that can be used when undertaking a Qualitative analysis of a building. The guidelines also provide guidance on calculating a modified Ultimate Limit State capacity of the building which is much more accurate and can be used when undertaking a Quantitative analysis.

The New Zealand Society for Earthquake Engineering has proposed a way for classifying earthquake risk for existing buildings in terms of %NBS and this is shown in Figure 1 below.

Description	Grade	Risk	%NBS	Existing Building Structural Performance	Improvement of Structural Performance	
					Legal Requirement	NZSEE Recommendation
Low Risk Building	A or B	Low	Above 67	Acceptable (improvement may be desirable)	The Building Act sets no required level of structural improvement (unless change in use) This is for each TA to decide. Improvement is not limited to 34%NBS.	100%NBS desirable. Improvement should achieve at least 67%NBS
Moderate Risk Building	B or C	Moderate	34 to 66	Acceptable legally. Improvement recommended		Not recommended. Acceptable only in exceptional circumstances
High Risk Building	D or E	High	33 or lower	Unacceptable (Improvement)	Unacceptable	Unacceptable

**Figure 1 NZSEE Risk Classifications Extracted from table 2.2 of the NZSEE 2006 AISPBE**

Table 1 compares the percentage NBS to the relative risk of the building failing in a seismic event with a 10% risk of exceedance in 50 years (i.e. 0.2% in the next year). It is noted that the current seismic risk in Christchurch results in a 6% risk of exceedance in the next year.



<b>Percentage of New Building Standard (%NBS)</b>	<b>Relative Risk (Approximate)</b>
>100	<1 time
80-100	1-2 times
67-80	2-5 times
33-67	5-10 times
20-33	10-25 times
<20	>25 times

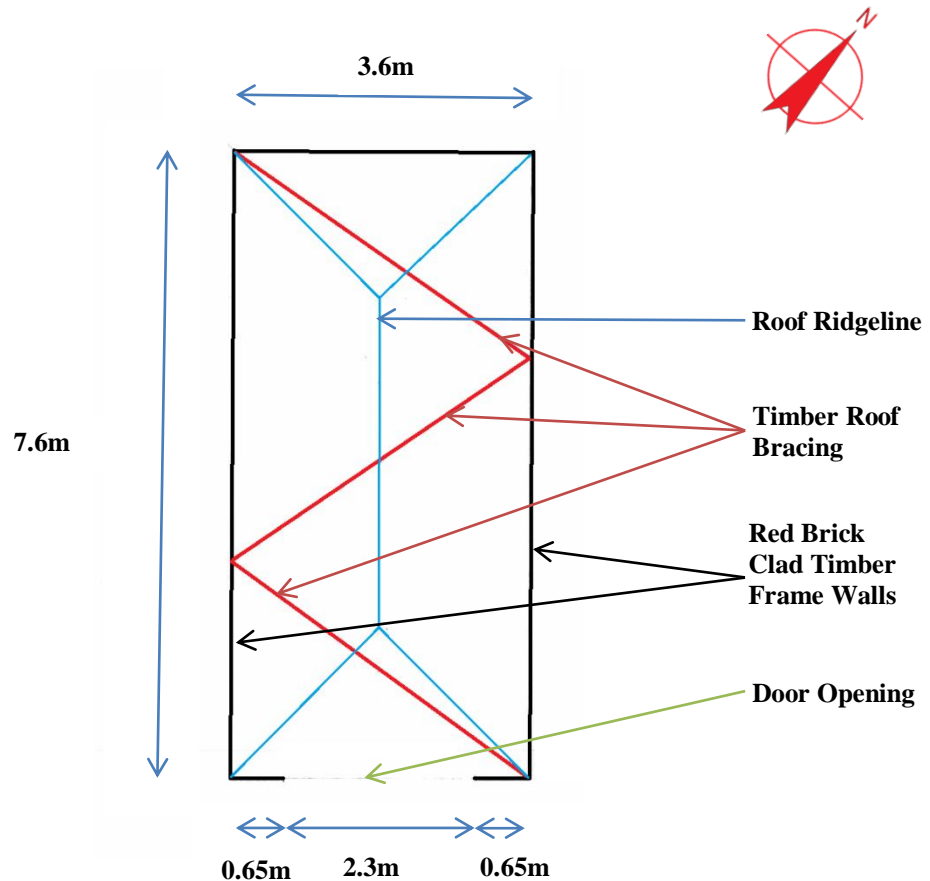
**Table 1 %NBS compared to relative risk of failure**

## 4. Building Description

### 4.1 General

The building is located at 75 Lower Styx Road, Bottle Lake, Christchurch. It was constructed in 1960 along with the adjacent dwelling. The building is currently used for storage purposes. There are 6 buildings located on the site with various uses.

The building is of lightweight timber frame construction, with red brick clad exterior walls. Wall ties tie the brick cladding to the timber wall studs. The roof structure consists of lightweight metal roofing fixed to timber sarking on timber roof trusses, supported at either end by the external load bearing timber framed walls. Foundations are concrete slab on grade.



**Figure 2 Plan Sketch Showing Key Structural Elements**

The dimensions of the building are approximately 7.6m in length, 3.6m in width and 3.75m in height. The plan area of the building is approximately 28m<sup>2</sup>.

The nearest building to the garage is the dwelling approximately 3m to the south. The closest waterway is the Styx River approximately 92m to the south-west. The site is predominantly flat with insignificant variations in the ground levels throughout.



## **4.2 Gravity Load Resisting System**

Gravity loads are transferred from the metal roof cladding to the timber sarking and into the timber roof trusses. The roof trusses span between the external load bearing timber framed walls. The load bearing timber framed walls transfer the gravity loads to the ground slab.

## **4.3 Lateral Load Resisting System**

Lateral load resisting systems in both the longitudinal and transverse direction are similar.

The roof structure is braced by the timber sarking and diagonal timber bracing members. This system transfers lateral loads to the perimeter walls. The perimeter walls have diagonal timber bracing that transfers lateral loads to the ground slab.



## 5. Assessment

An inspection of the building was undertaken on the 20<sup>th</sup> of April 2012. Both the interior and exterior of the building were inspected.

The inspection consisted of scrutinising the building to determine the structural systems and likely behaviour of the building during an earthquake. The site was assessed for damage, including examination of the ground conditions, checking for damage in areas where damage would be expected for the type of structure and noting general damage observed throughout the building in both structural and non-structural elements.

The %NBS score determined for this building has been based on the IEP procedure described by the NZSEE and based on the information obtained from visual observation of the building and available drawings.



## 6. Damage Assessment

### 6.1 Surrounding Buildings

The garage at 75 Lower Styx Road is located in a rural area with 5 other buildings. These buildings are a dwelling to the south, a hay barn, a storage shed and a barn to the west and a dairy unit to the south west. There did not appear to be any damage to any of these structures.

### 6.2 Residual Displacements and General Observations

No residual displacements of the structure were noticed during our inspection of the building.

No damage was noted to the building.

### 6.3 Ground Damage

There was no evidence of ground damage on the property. Neighbouring land to the southwest, at 51 Lower Styx Road, was severely affected by lateral spreading.





## 7. Critical Structural Weakness

### 7.1 Short Columns

No significant short columns are present in the structure.

### 7.2 Lift Shaft

The building does not contain a lift shaft.

### 7.3 Roof

The roof is braced by timber sarking and diagonal timber bracing.

### 7.4 Staircases

The building does not contain a staircase.

### 7.5 Site Characteristics

Following the geotechnical appraisal it was found that the site has a moderate to high potential for liquefaction. As the building has a concrete raft foundation, the effects of liquefaction are not likely to adversely affect the structure of the building. For the purposes of the IEP assessment of the building and the determination of the %NBS score, the effects of soil liquefaction on the performance of the building has been assessed as an 'insignificant' site characteristic in accordance with the NZSEE guidelines.

### 7.6 Plan Irregularity

There is a large opening in one of the short walls of the building; however the roof structure has adequate bracing in the form of timber sarking and diagonal timber roof bracing. Therefore the building is not considered to have a plan irregularity critical structural weakness.



## 8. Geotechnical Consideration

### 8.1 Site Description

The site at 75 Lower Styx Road is situated in Bottle Lake, just north of Christchurch City. It is situated between the Styx River and Lower Styx Road, and is relatively flat at approximately 3m above mean sea level. It is approximately 4km south of the Waimakariri River, and 4km west of the coast (Pegasus Bay).

### 8.2 Published Information on Ground Conditions

#### 8.2.1 Published Geology

The geological map of the area<sup>1</sup> indicates that the site to be on or near the boundary of the following units:

- grey river alluvium, comprising gravel, sand and silt, in active floodplains, Holocene in age; and,
- beach sand or river sand dunes, Holocene in age.

#### 8.2.2 Environment Canterbury Logs

Information from Environment Canterbury (ECan) indicates that three boreholes are located within a 200m radius of the site. Of these boreholes, two of them had lithographic logs (see Table 2), which indicate the area is typically underlain by 30m of sand and gravel. The logs also indicate the potential presence of strata containing peat at approximately 27m below ground level (bgl).

**Table 2 ECan Borehole Summary**

Bore Name	Log Depth	Groundwater	Distance & Direction from Site
M35/9747	30.0m	0.30m bgl	150m NE of the site
M35/11929	32.5m	0.72m bgl	150m SW of the site

It should be noted that the purpose of the boreholes the well logs are associated with, were sunk for groundwater extraction and not for geotechnical purposes. Therefore, the amount of material recovered and available for interpretation and recording will have been variable at best and may not be representative. The logs have been written by the well driller and not a geotechnical professional or to a standard. In addition strength data is not recorded.

#### 8.2.3 EQC Geotechnical Investigations

The Earthquake Commission has not undertaken geotechnical testing in the area of the subject site.

<sup>1</sup> Forsyth P.J., Barrell D.J.A., & Jongens R. 2008: *Geology of the Christchurch Area*. Institute of Geological and Nuclear Sciences 1:250,000 Geological Map 16. Lower Hutt. Institute of Geological and Nuclear Sciences Limited.

#### 8.2.4 Land Zoning

Canterbury Earthquake Recovery Authority (CERA) has published areas showing the Green Zone Technical Category in relation to the risk of future liquefaction and how these areas are expected to perform in future earthquakes.

The site is classified as Green Zone, indicating the land is generally suitable for repair and rebuilding to take place. It is also categorised Technical Category Not Applicable (rural & unmapped), as the property is considered non-residential.

#### 8.2.5 Post February Aerial Photography

Aerial photography taken following the 22 February 2011 earthquake doesn't show clear signs of liquefaction (see Figure 3), however, liquefaction was observed close to the property.

**Figure 3 Post February 2011 Earthquake Aerial Photography <sup>2</sup>**



#### 8.2.6 Summary of Ground Conditions

From the information presented above, the ground conditions underlying the site are anticipated to comprise beach sands and gravels. However, due to the limited information available, this is subject to confirmation.

<sup>2</sup> Aerial Photography Supplied by Koordinates sourced from <http://koordinates.com/layer/3185-christchurch-post-earthquake-aerial-photos-24-feb-2011/>



## 8.3 Seismicity

### 8.3.1 Nearby Faults

There are many faults in the Canterbury region, however only those considered most likely to have an adverse effect on the site are detailed below.

**Table 3 Summary of Known Active Faults<sup>34</sup>**

Known Active Fault	Distance from Site	Direction from Site	Max Likely Magnitude	Avg Recurrence Interval
Alpine Fault	130 km	NW	~8.3	~300 years
Greendale (2010) Fault	30 km	SW	7.1	~15,000 years
Hope Fault	100 km	N	7.2~7.5	120~200 years
Kelly Fault	110 km	NW	7.2	150 years
Porters Pass Fault	60 km	NW	7.0	1100 years

Recent earthquakes since 22 February 2011 have identified the presence of a previously unmapped active fault system underneath Christchurch City and the Port Hills. Research and published information on this system is in development and not generally available. Average recurrence intervals are yet to be estimated.

### 8.3.2 Ground Shaking Hazard

This seismic activity has produced earthquakes of Magnitude-6.3 with peak ground accelerations (PGA) up to twice the acceleration due to gravity (2g) in some parts of the city. This has resulted in widespread liquefaction throughout Christchurch.

New Zealand Standard NZS 1170.5:2004 quantifies the Seismic Hazard factor for Christchurch as 0.30, being in a moderate to high earthquake zone. This value has been provisionally upgraded recently (from 0.22) to reflect the seismicity hazard observed in the earthquakes since 4 September 2010.

In addition, anticipation of marine and/or estuarine sands of varying density, a 475-year PGA (peak ground acceleration) of ~0.4 (Stirling et al, 2002<sup>4</sup>), and bedrock anticipated to be in excess of 500m deep, and hence ground shaking is likely to be relatively high.

## 8.4 Slope Failure and/or Rockfall Potential

Given the site's location in a flat area northeast of Christchurch, global slope instability is considered negligible. However, the Styx River may be susceptible to lateral spreading, as evident to the north in Spencerville following the 4<sup>th</sup> September 2010 and 22<sup>nd</sup> February 2011 earthquakes.

<sup>3</sup> Stirling, M.W, McVerry, G.H, and Berryman K.R. (2002) A New Seismic Hazard Model for New Zealand, Bulletin of the Seismological Society of America, Vol. 92 No. 5, pp 1878-1903, June 2002.

<sup>4</sup> GNS Active Faults Database



In addition, any localised retaining structures or embankments should be further investigated to determine the site-specific slope instability potential.

## **8.5 Liquefaction Potential**

Due to the anticipated presence of sand at the shallow depth, and the sites proximity to a local water course, it is considered possible that liquefaction will occur at the site. It is not clear from the post-earthquake aerial photography (Figure 3) whether liquefaction has occurred at the site, however, it was observed close to the property. The site is considered to have a moderate-high liquefaction potential at this stage of investigation.

This liquefaction may occur in the form of sand boils, lateral spreading or both.

Ground Investigation should be undertaken to establish the liquefaction potential of the site and allow a comprehensive liquefaction assessment to be undertaken.

## **8.6 Recommendations**

A soil class of **D** (in accordance with NZS 1170.5:2004) should be adopted for the site.

It is recommended that three piezocone CPTs be conducted to target depths of 20m. This will allow a liquefaction assessment to be carried out.

## **8.7 Conclusions & Summary**

This assessment is based on a review of the geology and existing ground investigation information, and observations from the Christchurch earthquakes since 4 September 2010.

The site is anticipated to be situated predominantly on sand and gravel. Associated with this the site also has a moderate-high liquefaction potential. This may also arise in the form of lateral spreading on the Styx River.

A soil class of **D** (in accordance with NZS 1170.5:2004) should be adopted for the site.

It is recommended that intrusive investigation comprising three piezocone CPTs be conducted to target depths of 20m.



## 9. Survey

No level or verticality surveys have been undertaken for this building at this stage in accordance with Christchurch City Council guidelines.



## 10. Initial Capacity Assessment

### 10.1 % NBS Assessment

The building's capacity was assessed using the Initial Evaluation Procedure based on the information available. The building's capacity excluding critical structural weaknesses and the capacity of any identified weaknesses are expressed as a percentage of new building standard (%NBS) and are in the order of that shown below in Table 4. These capacities are subject to confirmation by a more detailed quantitative analysis.

<u>Item</u>	<u>%NBS</u>
Building excluding CSW's	43

**Table 4 Indicative Building and Critical Structural Weaknesses Capacities based on the NZSEE Initial Evaluation Procedure**

Following an IEP assessment, the building has been assessed as achieving 43% New Building Standard (NBS). Under the New Zealand Society for Earthquake Engineering (NZSEE) guidelines the building is considered potentially Earthquake Risk as it achieves less than 67% NBS. This score has not been adjusted when considering damage to the structure as no damage was seen.

### 10.2 Seismic Parameters

The seismic design parameters based on current design requirements from NZS 1170:2002 and the NZBC clause B1 for this building are:

- Site soil class: D, NZS 1170.5:2004, Clause 3.1.3, Soft Soil
- Site hazard factor,  $Z = 0.3$ , NZBC, Clause B1 Structure, Amendment 11 effective from 1 August 2011
- Return period factor  $R_u = 0.5$ , NZS 1170.5:2004, Table 3.5, Importance level 1 structure with a 50 year design life.

An increased  $Z$  factor of 0.3 for Christchurch has been used in line with requirements from the Department of Building and Housing resulting in a reduced % NBS score.

### 10.3 Expected Structural Ductility Factor

A structural ductility factor of 2 has been assumed based on the structural system observed and the date of construction.

### 10.4 Discussion of Results

The results obtained from the initial IEP assessment are consistent with those expected for a building of this age and construction type. The building was constructed in 1960 and would have been designed to the standards at the time, NZS 1900. These standards would have used design loads significantly less than those required by current loading standards and lower detailing requirements for ductile seismic behaviour than those that are present in current standards. When combined with the increase in the hazard factor for Christchurch to 0.3, it would be expected that the building would not achieve 100% NBS and it is reasonable to expect the building to be classified as potentially Earthquake Risk.



## **10.5 Occupancy**

As the building has been found to have a % NBS greater than 33% but less than 67%, it is deemed as potentially Earthquake Risk. Occupancy of the building is not restricted as the building has achieved greater than 34% NBS.





## 11. Initial Conclusions

The building has been assessed to have a seismic capacity in the order of 43% NBS and is therefore potentially Earthquake Risk in accordance with the NZSEE guidelines. The building can remain occupied, as the building is not considered to be Earthquake Prone.



## 12. Recommendations

The recent seismic activity in Christchurch has caused no visible damage to the building. But as the building has achieved less than 67% NBS following a qualitative Detailed Engineering Evaluation of the building, GHD recommend that a more detailed quantitative assessment and geotechnical investigation is carried out and if necessary strengthening options explored.

As the building has achieved greater than 34% NBS, it can remain occupied.



## 13. Limitations

### 13.1 General

This report has been prepared subject to the following limitations:

- ▶ No intrusive structural investigations have been undertaken.
- ▶ No intrusive geotechnical investigations have been undertaken.
- ▶ No level or verticality surveys have been undertaken.
- ▶ No material testing has been undertaken.
- ▶ No calculations, other than those included as part of the IEP in the CERA Building Evaluation Report, have been undertaken. No modelling of the building for structural analysis purposes has been performed.

It is noted that this report has been prepared at the request of Christchurch City Council and is intended to be used for their purposes only. GHD accepts no responsibility for any other party or person who relies on the information contained in this report or a specific limitations section.

### 13.2 Geotechnical Limitations

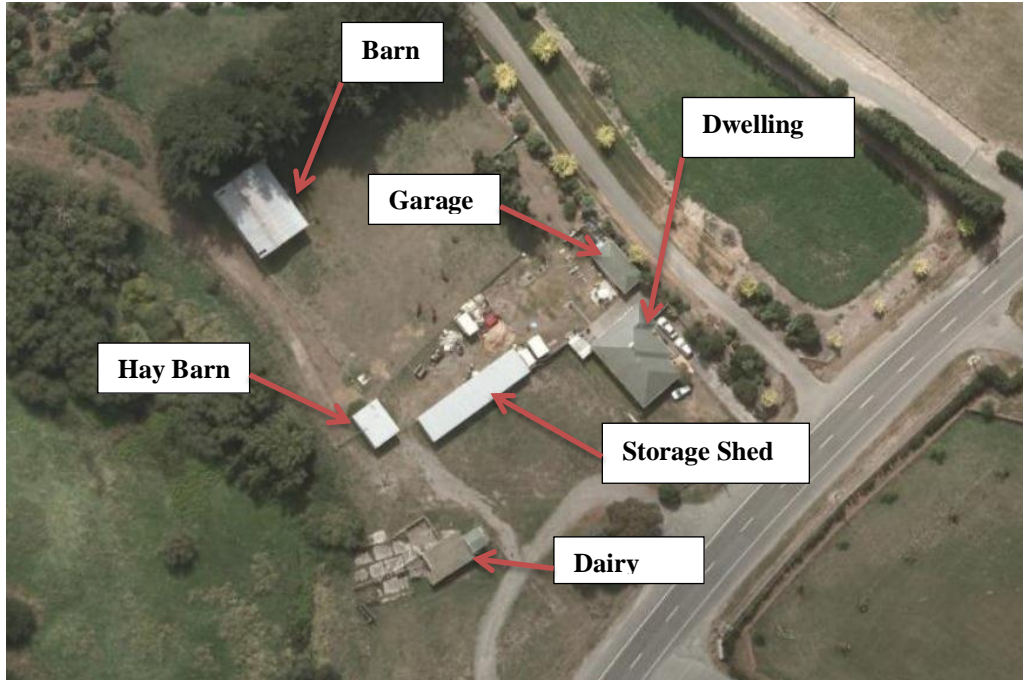
This report presents the results of a geotechnical appraisal prepared for the purpose of this commission, and for prepared solely for the use of Christchurch City Council and their advisors. The data and advice provided herein relate only to the project and structures described herein and must be reviewed by a competent geotechnical engineer before being used for any other purpose. GHD Limited (GHD) accepts no responsibility for other use of the data.

The advice tendered in this report is based on a visual geotechnical appraisal. No subsurface investigations have been conducted. An assessment of the topographical land features have been made based on this information. It is emphasised that Geotechnical conditions may vary substantially across the site from where observations have been made. Subsurface conditions, including groundwater levels can change in a limited distance or time. In evaluation of this report cognisance should be taken of the limitations of this type of investigation.

An understanding of the geotechnical site conditions depends on the integration of many pieces of information, some regional, some site specific, some structure specific and some experienced based. Hence this report should not be altered, amended or abbreviated, issued in part and issued incomplete in any way without prior checking and approval by GHD. GHD accepts no responsibility for any circumstances, which arise from the issue of the report, which have been modified in any way as outlined above.



Appendix A  
Photographs



**Photograph 1 Aerial photograph of site at 75 Lower Styx Road.**



**Photograph 2 View of the garage from the southeast.**



**Photograph 3 View of the garage from the southwest.**



**Photograph 4 View of the garage from the northwest.**



**Photograph 5** View of the interior at the western corner of the building. No internal wall linings. Roof and wall bracing is clearly visible.



**Photograph 6** Internal view of the southern corner of the building showing wall bracing.



**Photograph 7 Timber roof bracing and roof trusses visible.**

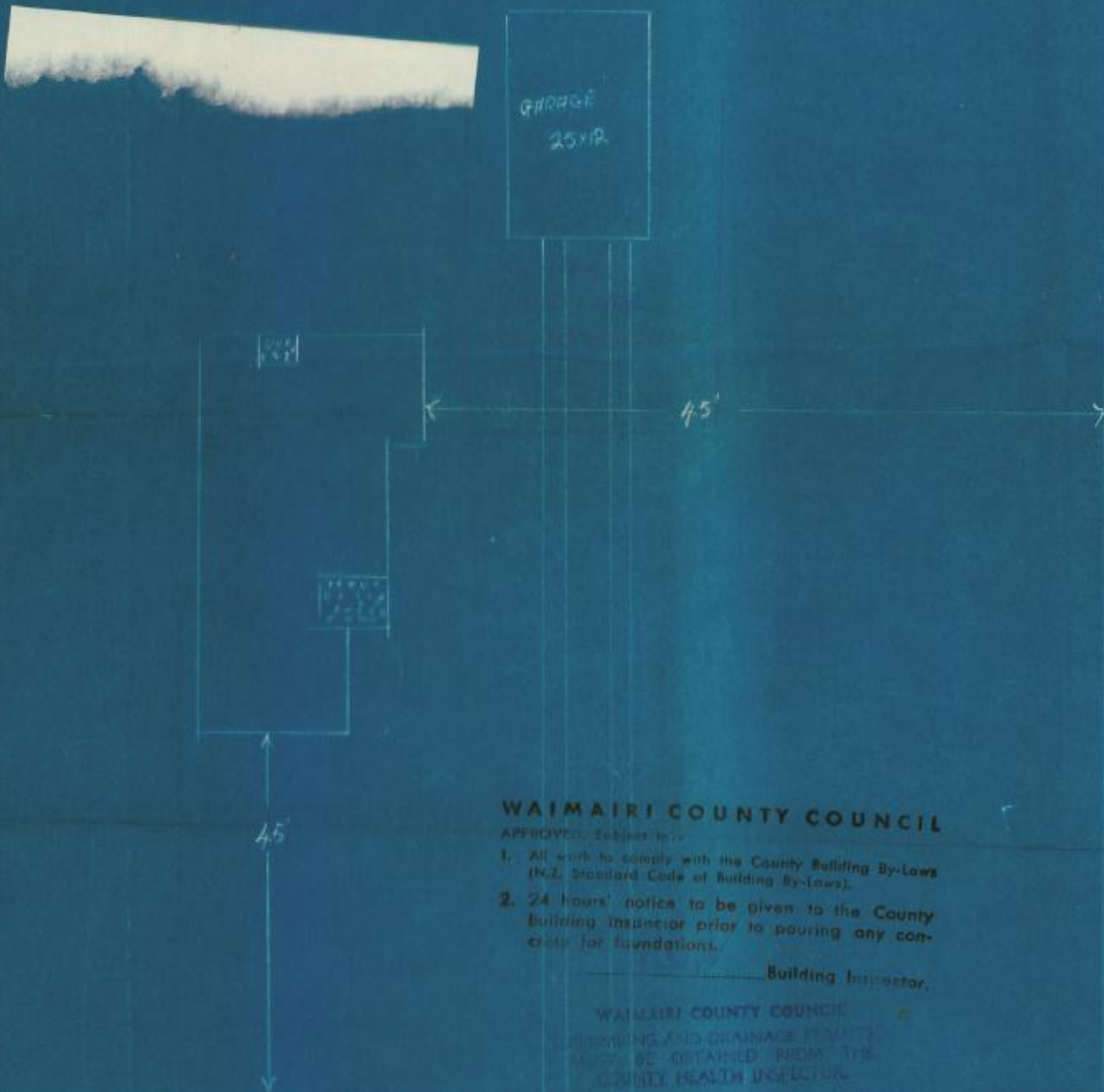


**Photograph 8 Timber roof sarking and wall bracing clearly visible.**





Appendix B  
Existing Drawings

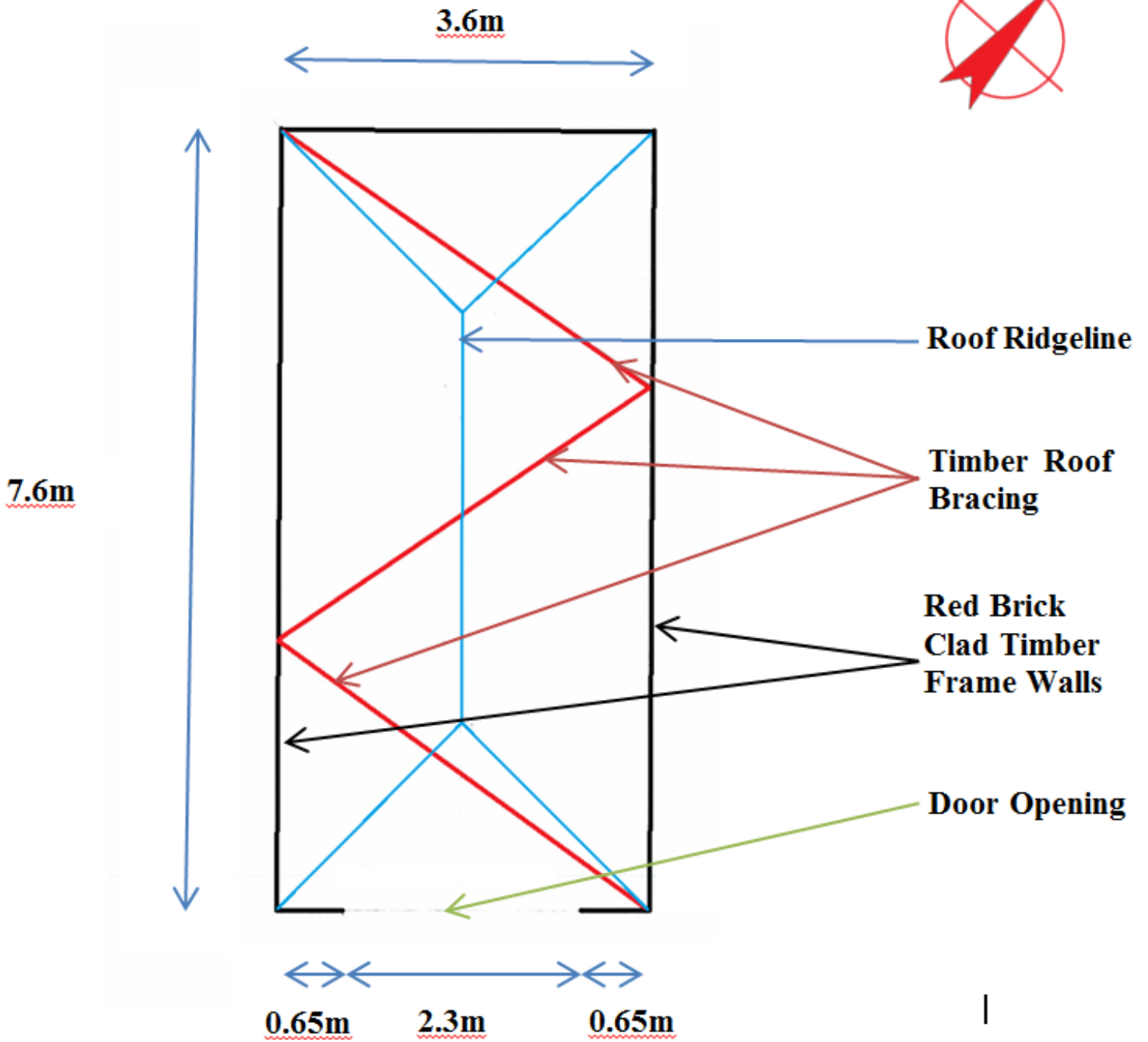


**WAIMAIRI COUNTY COUNCIL**  
APPROVED, Subject to:-

1. All work to comply with the County Building By-Laws (N.Z. Standard Code of Building By-Laws).
2. 24 hours' notice to be given to the County Building Inspector prior to pouring any concrete for foundations.

\_\_\_\_\_  
Building Inspector,  
WAIMAIRI COUNTY COUNCIL  
BUILDINGS AND DRAINAGE DEPARTMENT  
OFFICE BE OBTAINED FROM THE  
COUNTY HEALTH INSPECTOR.

75' LOWER STYX RD.





Appendix C  
CERA Building Evaluation Form

Detailed Engineering Evaluation Summary Data

V1.11

<b>Location</b>		Building Name: <input type="text" value="Garage"/>	Reviewer: <input type="text" value="Stephen Lee"/>
	Unit No: Street		CPEng No: <input type="text" value="1006840"/>
Building Address: <input type="text" value="2 75 Lower Styx Road"/>			Company: <input type="text" value="GHD"/>
Legal Description: <input type="text" value="Lot 1 DP 4410"/>			Company project number: <input type="text" value="513059665"/>
			Company phone number: <input type="text" value="(03) 3780900"/>
	Degrees Min Sec		Date of submission: <input type="text" value="14/05/2013"/>
GPS south: <input type="text" value="43 27 31.46"/>			Inspection Date: <input type="text" value="20/04/12"/>
GPS east: <input type="text" value="172 39 58.84"/>			Revision: <input type="text" value="Final"/>
Building Unique Identifier (CCC): <input type="text" value="PRK 2561 BLDG 002"/>			Is there a full report with this summary? <input type="text" value="yes"/>

<b>Site</b>		Site slope: <input type="text" value="flat"/>	Max retaining height (m): <input type="text" value="0"/>
	Soil type: <input type="text" value="mixed"/>		Soil Profile (if available): <input type="text" value="Gravel Sand and Silt"/>
Site Class (to NZS1170.5): <input type="text" value="D"/>			
Proximity to waterway (m, if <100m): <input type="text" value="92"/>		If Ground improvement on site, describe: <input type="text" value="n/a"/>	
Proximity to clifftop (m, if < 100m): <input type="text"/>			
Proximity to cliff base (m,if <100m): <input type="text"/>		Approx site elevation (m): <input type="text" value="3.00"/>	

<b>Building</b>		No. of storeys above ground: <input type="text" value="1"/>	single storey = 1	Ground floor elevation (Absolute) (m): <input type="text" value="3.00"/>
	Ground floor split? <input type="text" value="no"/>			Ground floor elevation above ground (m): <input type="text" value="0.00"/>
	Stores below ground: <input type="text" value="0"/>			
	Foundation type: <input type="text" value="mat slab"/>		if Foundation type is other, describe: <input type="text"/>	
	Building height (m): <input type="text" value="3.75"/>	height from ground to level of uppermost seismic mass (for IEP only) (m): <input type="text" value="3.5"/>		
	Floor footprint area (approx): <input type="text" value="28"/>			
	Age of Building (years): <input type="text" value="52"/>			Date of design: <input type="text" value="1935-1965"/>
	Strengthening present? <input type="text" value="no"/>			If so, when (year)? <input type="text"/>
	Use (ground floor): <input type="text" value="parking"/>			And what load level (%g)? <input type="text"/>
	Use (upper floors): <input type="text"/>			Brief strengthening description: <input type="text"/>
	Use notes (if required): <input type="text" value="Car Garage"/>			
	Importance level (to NZS1170.5): <input type="text" value="IL1"/>			

<b>Gravity Structure</b>		Gravity System: <input type="text" value="load bearing walls"/>	truss depth, purlin type and cladding: <input type="text"/>
	Roof: <input type="text" value="timber truss"/>		slab thickness (mm): <input type="text"/>
	Floors: <input type="text" value="concrete flat slab"/>		overall depth x width (mm x mm): <input type="text"/>
	Beams: <input type="text" value="none"/>		
	Columns: <input type="text"/>		None
	Walls: <input type="text"/>		Timber Framed Walls

<b>Lateral load resisting structure</b>		Lateral system along: <input type="text" value="lightweight timber framed walls"/>	<b>Note: Define along and across in</b>	note typical wall length (m) <input type="text"/>
---	--	--	---	---

Ductility assumed, $\mu$ :	2.00
Period along:	0.40
Total deflection (ULS) (mm):	
maximum interstorey deflection (ULS) (mm):	
Lateral system across:	lightweight timber framed walls
Ductility assumed, $\mu$ :	2.00
Period across:	0.40
Total deflection (ULS) (mm):	
maximum interstorey deflection (ULS) (mm):	

**detailed report!**

0.00

estimate or calculation?  
estimate or calculation?  
estimate or calculation?


Lateral system across:	lightweight timber framed walls
Ductility assumed, $\mu$ :	2.00
Period across:	0.40
Total deflection (ULS) (mm):	
maximum interstorey deflection (ULS) (mm):	

0.00

note typical wall length (m)  
estimate or calculation?  
estimate or calculation?  
estimate or calculation?


**Separations:**

north (mm):	
east (mm):	
south (mm):	
west (mm):	

leave blank if not relevant

**Non-structural elements**

Stairs:	
Wall cladding:	brick or tile
Roof Cladding:	Metal
Glazing:	timber frames
Ceilings:	none
Services(list):	

describe (note cavity if exists)  
describe

Brick exterior 75mm cavity
Lightweight Metal Cladding

**Available documentation**

Architectural	none
Structural	none
Mechanical	none
Electrical	none
Geotech report	none

original designer name/date  
original designer name/date  
original designer name/date  
original designer name/date  
original designer name/date

Unknown, 1960

**Damage**

Site:  
(refer DEE Table 4-2)

Site performance:	Good
Settlement:	none observed
Differential settlement:	none observed
Liquefaction:	none apparent
Lateral Spread:	none apparent
Differential lateral spread:	none apparent
Ground cracks:	none apparent
Damage to area:	none apparent

Describe damage:

--

notes (if applicable):  
notes (if applicable):  
notes (if applicable):  
notes (if applicable):  
notes (if applicable):  
notes (if applicable):


**Building:**

Current Placard Status:	green
-------------------------	-------

Along

Damage ratio:	0%
Describe (summary):	No damage

Describe how damage ratio arrived at:

--

Across

Damage ratio:	0%
Describe (summary):	No damage

$$Damage\_Ratio = \frac{(\% NBS (before) - \% NBS (after))}{\% NBS (before)}$$

Diaphragms	Damage?:	<input type="text" value="no"/>	Describe:	<input type="text"/>
CSWs:	Damage?:	<input type="text" value="no"/>	Describe:	<input type="text"/>
Pounding:	Damage?:	<input type="text" value="no"/>	Describe:	<input type="text"/>
Non-structural:	Damage?:	<input type="text" value="no"/>	Describe:	<input type="text"/>

**Recommendations**

Level of repair/strengthening required:	<input type="text" value="none"/>	Describe:	<input type="text"/>
Building Consent required:	<input type="text" value="no"/>	Describe:	<input type="text"/>
Interim occupancy recommendations:	<input type="text" value="do not occupy"/>	Describe:	<input type="text"/>

Along	Assessed %NBS before:	<input type="text" value="21%"/>	43% %NBS from IEP below	If IEP not used, please detail assessment methodology:	<input type="text"/>
	Assessed %NBS after:	<input type="text" value="21%"/>			
Across	Assessed %NBS before:	<input type="text" value="21%"/>	43% %NBS from IEP below		
	Assessed %NBS after:	<input type="text" value="21%"/>			

**IEP** Use of this method is not mandatory - more detailed analysis may give a different answer, which would take precedence. Do not fill in fields if not using IEP.

Period of design of building (from above): 1935-1965 h<sub>n</sub> from above: 3.5m

Seismic Zone, if designed between 1965 and 1992:

	along	across
Period (from above):	0.4	0.4
(%NBS) <sub>nom</sub> from Fig 3.3:	<input type="text" value="2.9%"/>	<input type="text" value="2.9%"/>

Note:1 for specifically design public buildings, to the code of the day: pre-1965 = 1.25; 1965-1976, Zone A = 1.33; 1965-1976, Zone B = 1.2; all else 1.0

Note 2: for RC buildings designed between 1976-1984, use 1.2

Note 3: for buildings designed prior to 1935 use 0.8, except in Wellington (1.0)

	along	across
<b>Final (%NBS)<sub>nom</sub>:</b>	<input type="text" value="3%"/>	<input type="text" value="3%"/>

**2.2 Near Fault Scaling Factor**

Near Fault scaling factor, from NZS1170.5, cl 3.1.6:

	along	across
Near Fault scaling factor (1/N(T,D), <b>Factor A:</b>	<input type="text" value="1"/>	<input type="text" value="1"/>

**2.3 Hazard Scaling Factor**

Hazard factor Z for site from AS1170.5, Table 3.3:

Z<sub>1992</sub>, from NZS4203:1992

Hazard scaling factor, **Factor B:**

**2.4 Return Period Scaling Factor**

Building Importance level (from above):

Return Period Scaling factor from Table 3.1, **Factor C:**

**2.5 Ductility Scaling Factor**

Assessed ductility (less than max in Table 3.2)  
 Ductility scaling factor: =1 from 1976 onwards; or = $\kappa\mu$ , if pre-1976, from Table 3.3:

	along	across
Assessed ductility (less than max in Table 3.2)	2.00	2.00
Ductility scaling factor: =1 from 1976 onwards; or = $\kappa\mu$ , if pre-1976, from Table 3.3:	1.57	1.57

Ductility Scaling Factor, **Factor D:**

	1.57	1.57
--	------	------

**2.6 Structural Performance Scaling Factor:**

Sp:

	0.700	0.700
--	-------	-------

Structural Performance Scaling Factor **Factor E:**

	1.428571429	1.428571429
--	-------------	-------------

**2.7 Baseline %NBS,  $(NBS\%)_b = (\%NBS)_{nom} \times A \times B \times C \times D \times E$**

%NBS<sub>b</sub>:

	43%	43%
--	-----	-----

Global Critical Structural Weaknesses: (refer to NZSEE IEP Table 3.4)

**3.1. Plan Irregularity, factor A:**

insignificant	1
---------------	---

**3.2. Vertical irregularity, Factor B:**

insignificant	1
---------------	---

**3.3. Short columns, Factor C:**

insignificant	1
---------------	---

**3.4. Pounding potential**

Pounding effect D1, from Table to right	1.0
Height Difference effect D2, from Table to right	1.0

Therefore, Factor D: 1

**3.5. Site Characteristics**

insignificant	1
---------------	---

Table for selection of D1	Severe	Significant	Insignificant/none
	0<sep<.005H	.005<sep<.01H	Sep>.01H
Separation			
Alignment of floors within 20% of H	0.7	0.8	1
Alignment of floors not within 20% of H	0.4	0.7	0.8

Table for Selection of D2	Severe	Significant	Insignificant/none
	0<sep<.005H	.005<sep<.01H	Sep>.01H
Separation			
Height difference > 4 storeys	0.4	0.7	1
Height difference 2 to 4 storeys	0.7	0.9	1
Height difference < 2 storeys	1	1	1

**3.6. Other factors, Factor F**

For ≤ 3 storeys, max value =2.5, otherwise max valule =1.5, no minimum  
 Rationale for choice of F factor, if not 1

	Along	Across
	1.0	1.0

Detail Critical Structural Weaknesses: (refer to DEE Procedure section 6)

List any:  Refer also section 6.3.1 of DEE for discussion of F factor modification for other critical structural weaknesses

**3.7. Overall Performance Achievement ratio (PAR)**

	1.00	1.00
--	------	------

**4.3 PAR x (%NBS)<sub>b</sub>:**

PAR x Baseline %NBS:

	43%	43%
--	-----	-----

**4.4 Percentage New Building Standard (%NBS), (before)**

	43%
--	-----

Official Use only:

Accepted By:   
 Date:







**GHD**

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**Document Status**

Rev No.	Author	Reviewer		Approved for Issue		
		Name	Signature	Name	Signature	Date
Final	Peter O'Brien	Cormac Joy		Stephen Lee		14/05/13